

 Project Locations

Project Location Map

CITY OF READING
 Public Works Department
 503 North Sixth St., Reading, PA 19601
2015 REPAVING PROJECT
Location Map
 NTS 7/7/15
 Drawing No. C 100

INDEX TO DRAWINGS

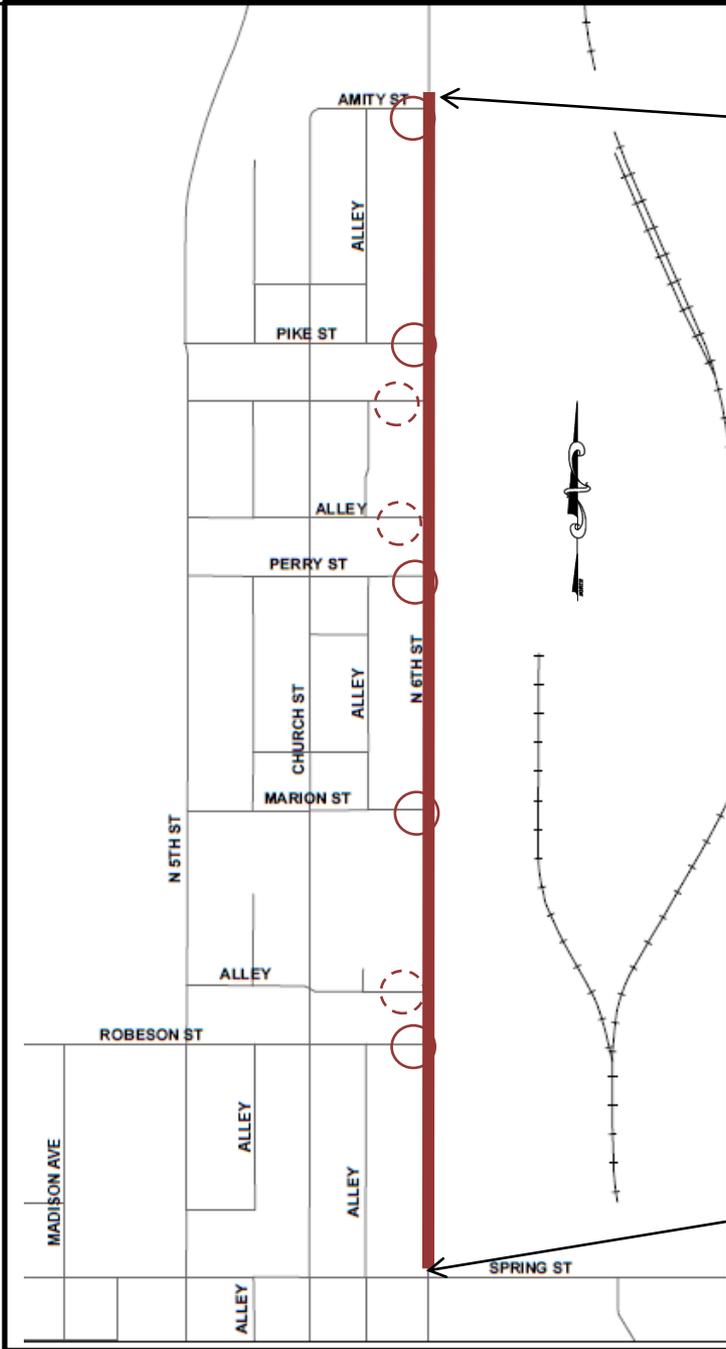
DRAWING NO.	DESCRIPTION
C100	LOCATION PLAN
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C103	S. 9TH STREET PLAN
C104	HIL ROAD PLAN
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PUBLIC UTILITIES

BELOW IS A LIST OF TYPICAL UTILITY PROVIDERS, NOT INCLUSIVE. It shall be the contractor's responsibility to verify the exact location of utilities prior to any earthmoving activities.

OWNER	ADDRESS	REPRESENTATIVE	TELEPHONE	RESPONSE
MET-ED (FIRST ENERGY)	2800 POTTSVILLE PIKE, PO BOX 16001, READING, PA	KENNY SWEITZER	610-921-6568	
CITY OF READING	503 N. 6TH ST., READING, PA 19601	TIM KRALL	610-655-1850	
READING AREA WATER AUTHORITY	1801 KUTZTOWN ROAD, READING, PA 19601	MARK KOZLOWSKI	610-655-6254	
UGI UTILITIES	225 MORGANTOWN ROAD, READING, PA 19611	KURT ZIELASKOWSKI	610- 736-5571	
VERIZON PENNSYLVANIA, INC.	1655 STATE HILL ROAD, READING, PA	JEFF KRAMER	(610) 371-4344	
COMCAST CABLE	400 RIVERFRONT DR., READING, PA, 19602	JEFF ROECKLE		

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Index / Act 187
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TIE PAVEMENT OVERLAY INTO EXISTING PAVEMENT JOINT AT THE NORTH SIDE OF THE INTERSECTION

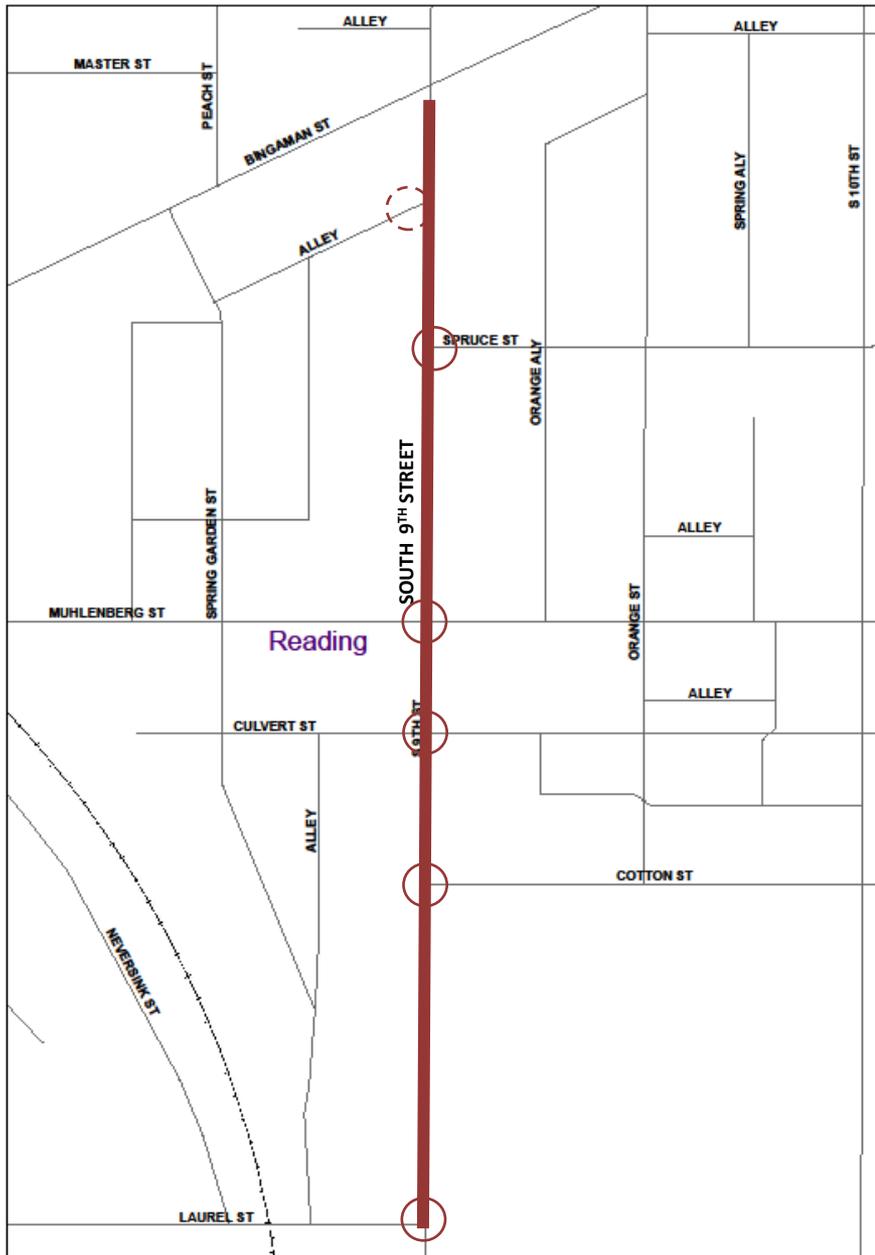
TIE OVERLAY INTO EXISTING PAVEMENT JOINT AT THE NORTH SIDE OF THE INTERSECTION

LEGEND

- Repaving Section
- Install Curb Ramps and Mark Crosswalks
- Reconstruct Alley Entrance

NORTH 6TH STREET PLAN

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N. 6th Street Plan
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 Drawing No. C 102



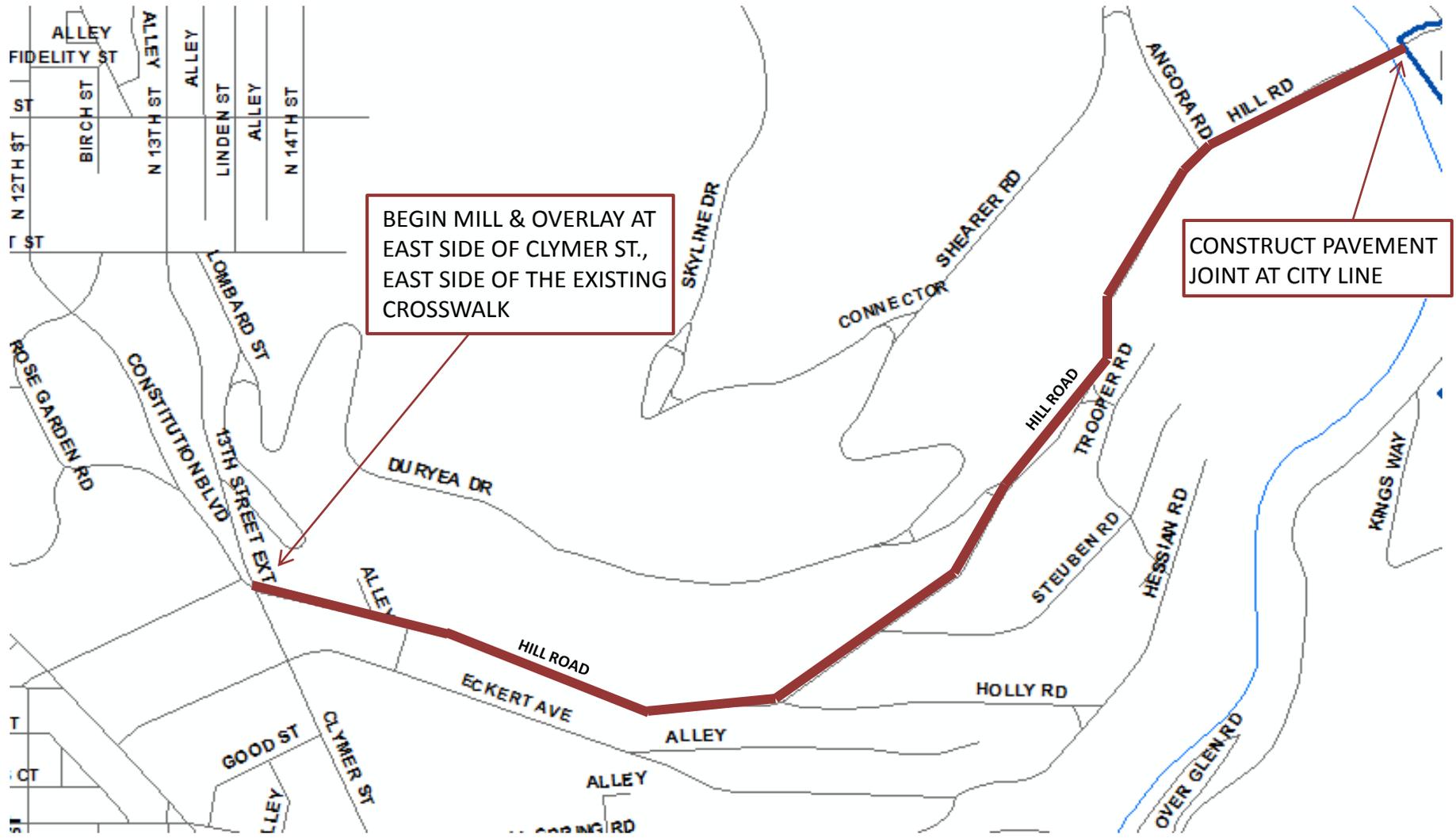
CONSTRUCT PAVEMENT JOINTS AT BOTH ENDS OF PAVEMENT SECTION

LEGEND

- Repaving Section
- Install Curb Ramps and Mark Crosswalks
- Reconstruct Alley Entrance

SOUTH 9TH STREET PLAN

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S. 9th Street Plan
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BEGIN MILL & OVERLAY AT EAST SIDE OF CLYMER ST., EAST SIDE OF THE EXISTING CROSSWALK

CONSTRUCT PAVEMENT JOINT AT CITY LINE

HILL ROAD PLAN

LEGEND

— Repaving Section

THERE ARE NO CROSSWALKS OR CURB RAMPS ANTICIPATED FOR THE HILL ROAD REPAVING

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HILL ROAD PLAN
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 Drawing No. C 104

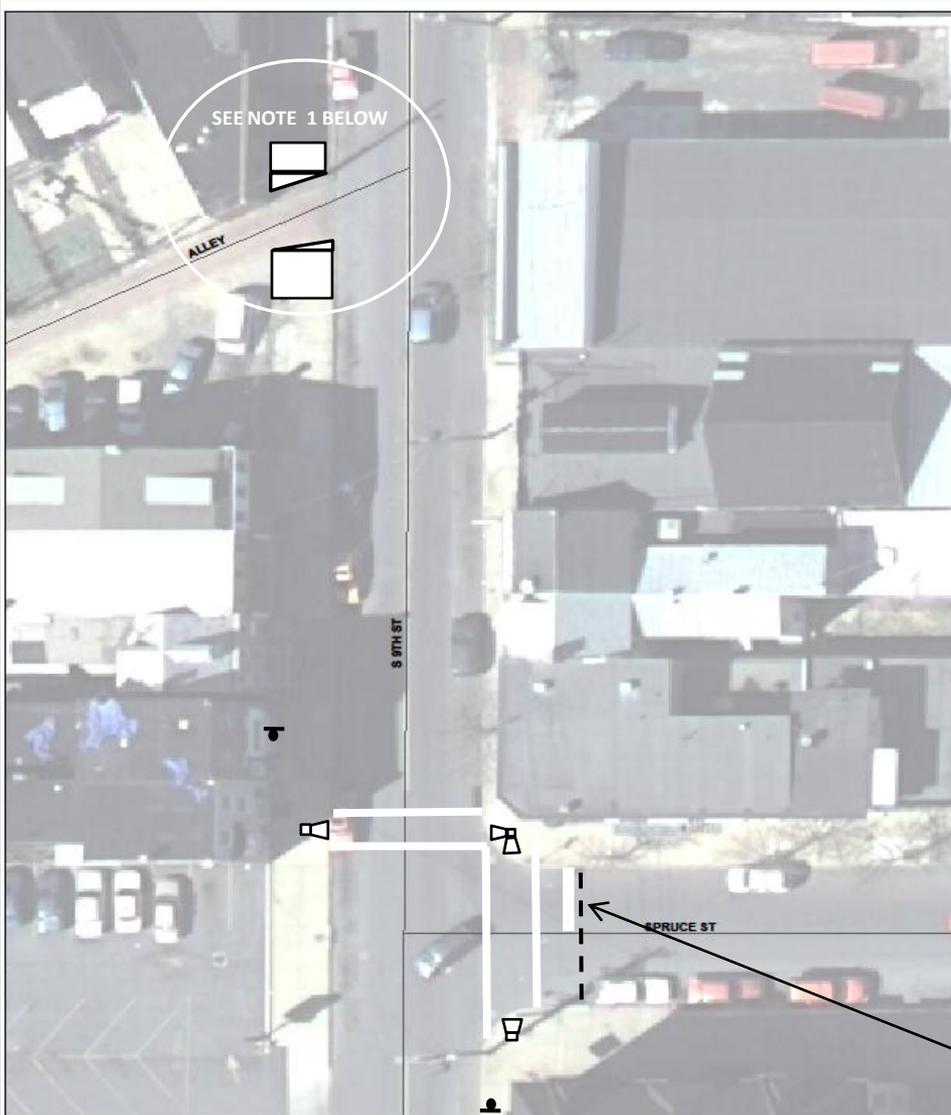
CONSTRUCTION NOTES:

1. ALIGNMENT OF THE CROSSWALKS AND CURB RAMPS AT STREET "T" INTERSECTIONS AND 4-WAY INTERSECTIONS SHALL BE AS SHOWN IN THE TYPICAL DETAILS. SITE CONDITIONS WILL REQUIRE VERTICAL AND HORIZONTAL FIELD ADJUSTMENTS WHICH SHALL BE ESTABLISHED IN THE FIELD BY THE CONTRACTOR FOR EACH CROSSWALK, AS APPROVED BY THE CITY.
2. ALIGNMENT AND EXTENT OF THE SIDEWALKS AND ALLEY ENTRANCE SECTIONS SHALL BE AS SHOWN IN THE TYPICAL DETAILS WITH THE FINAL ALIGNMENT TO BE ESTABLISHED IN THE FIELD BY THE CONTRACTOR, AS APPROVED BY THE CITY .
3. ALL CURB RAMP AND SIDEWALK RECONSTRUCTION WORK SHALL INCLUDE THE DEMOLITION OF THE EXISTING CURB , SIDEWALK AND STREET PAVEMENT AS NEEDED TO PROPERLY INSTALL THE WORK. ALL SIDEWALK DEMOLITION SHALL EXTEND TO THE NEAREST EXISTING SIDEWALK BLOCK JOINT. EDGES OF THE DEMOLITION SHALL BE TO NEAT LINES WITH SAW CUT JOINTS.
4. ALLEY ENTRANCE RECONSTRUCTION ALONG N. 6TH STREET SHALL INCLUDE THE DEMOLITION AND RECONSTRUCTION OF THE ENTRANCE SECTIONS OF THE ALLEYS AS SHOWN ON THE TYPICAL PLAN. THE ENTRANCE SHALL INCLUDE THE FULL WIDTH OF THE ALLEY WITHIN, BUT NOT INCLUDING, THE ALLEY CURBS , AND SHALL EXTEND FROM THE OUTSIDE EDGE OF THE SIDEWALK (THE RIGHT OF WAY LINE) TO THE FRONT EDGE OF THE ALLEY ENTRANCE, OR AS DIRECTED BY THE CITY. THE ALLEY ENTRANCE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF READING'S ENGINEERING STANDARDS FOR ALLEY CONSTRUCTION, AS REVISED, AND SHALL ALSO MEET THE ADA (PROWAG) ACCESSIBILITY GUIDELINES. ALL JOINTS AND BREAKS IN GRADE SHALL BE FLAT, SMOOTH AND FLUSH. THE JOINTS AND TRANSITIONS BETWEEN THE EXISTING SIDEWALK AND THE RECONSTRUCTED ALLEY SURFACES SHALL BE FLUSH, SMOOTH AND WITH NO VERTICAL LIPS OR SHARP GRADE BREAKS. DRAINAGE SHALL BE DIRECTED TO THE CENTER OF THE ALLEY SECTION AND SHAPED AS A PARABOLIC SWALE (NOT "V" SHAPED).
5. EXTEND THE MILL & OVERLAY AREAS INTO THE CROSSWALKS ON ALL SIDE STREETS AS SHOWN ON THE TYPICAL DETAILS.
6. ALL DIMENSIONS ARE APPROXIMATE. CONTRACTOR SHALL FIELD CONFIRM ALL DIMENSIONS.
7. CURB RAMPS SHALL BE DESIGNED AND CONSTRUCTED PER PENNDOT PUB 67M. ALL RAMPS SHALL BE TYPE 1 UNLESS CONDITIONS WARRANT ANOTHER STANDARD PENN DOT CURB RAMP TYPE. FINAL APPROVAL OF THE SELECTION SHALL BE AS DIRECTED BY THE CITY ENGINEER IN THE FIELD.
8. CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT CURB RAMPS AND CROSSWALKS ARE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE LATEST ADA / PENN DOT STANDARDS.
9. CURB RAMPS AND CROSSWALKS PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH PENN DOT STANDARD RC-67M, LATEST EDITION. STOP LINES PAVEMENT MARKINGS SHALL BE INSTALLED AT CROSSWALKS WITH STOP SIGNS.
10. PEDESTRIAN CROSSWALK SIGNS (MUTCD W11-2) SHALL BE INSTALLED AT THE APPROACH TO ALL UNCONTROLLED INTERSECTIONS (NO STOP SIGN INTERSECTIONS).
11. AS-BUILT PLANS OF ALL CURB RAMPS SHALL BE SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL.
12. ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH PENNDOT PUB 408.
13. ALL WORK SHALL COMPLYING WITH PENNSYLVANIA ACT 187, AS AMENDED BY ACT 181, AND SHALL CONTACT THE "PA ONE CALL SYSTEM" AT LEAST THREE WORKING DAYS PRIOR TO THE START OF CONSTRUCTION. BELOW IS A LIST OF SOME OF THE KNOWN UTILITIES AND THEIR CONTACTS . ADDITIONAL NOTIFICATIONS AND MARKINGS MAY BE NECESSARY AS THE PROJECT PROGRESSES.

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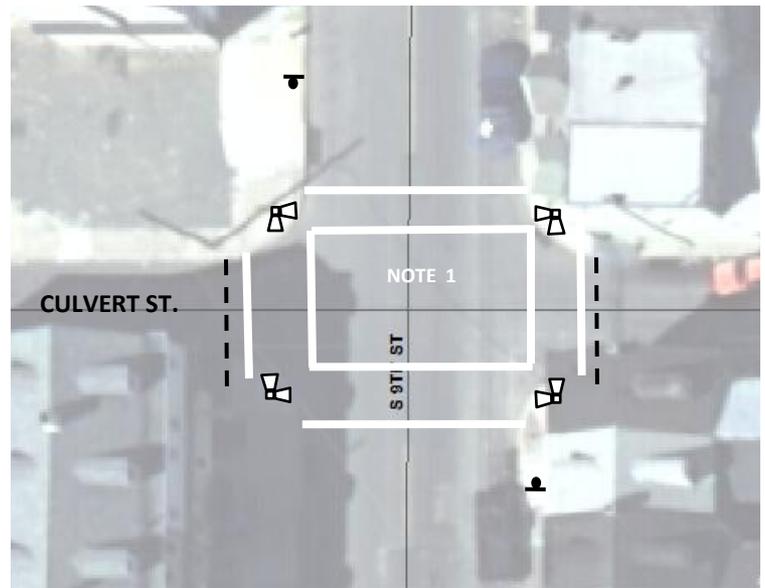
Notes

NTS 7/7/15
Drawing No. C 105



**TYPICAL CROSSWALK PLAN
"T" INTERSECTIONS AND ALLEYS**

NOTE 1: SIDEWALK SECTIONS AT ALLEY ENTRANCE SHALL INCLUDE THE REPLACEMENT OF THE CURBS THAT BORDER THE ALLEY. TOP OF CURBS SHALL BE SET FLUSH WITH THE EXISTING BRICK ALLEY. REPLACE ANY DISTURBED BRICK AS NECESSARY TO ACHIEVE A SMOOTH FLUSH SURFACE. LONGITUDINAL SLOPE SHALL BE 5% MAX. AND THE CROSS SLOPE SHALL BE 1.7%.



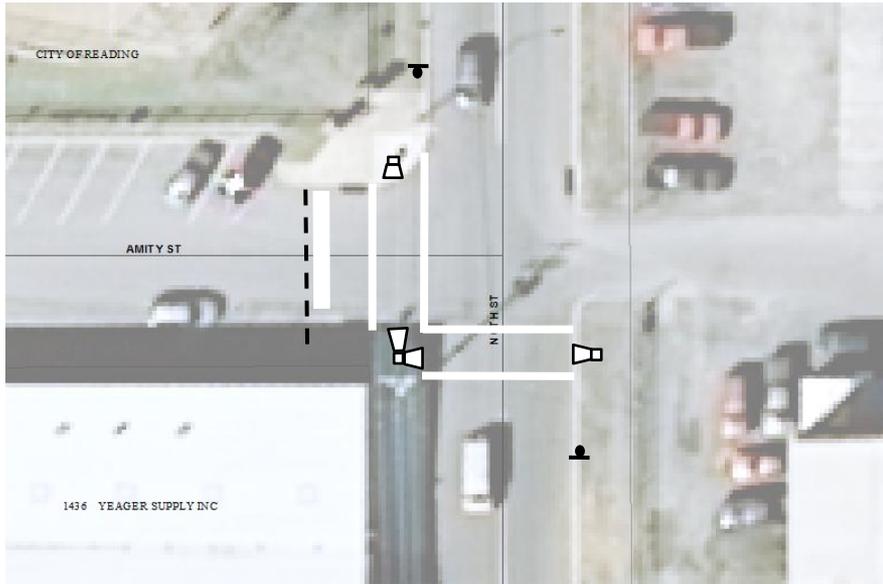
**TYPICAL CROSSWALK PLAN
4-WAY INTERSECTIONS - UNCONTROLLED**

LEGEND

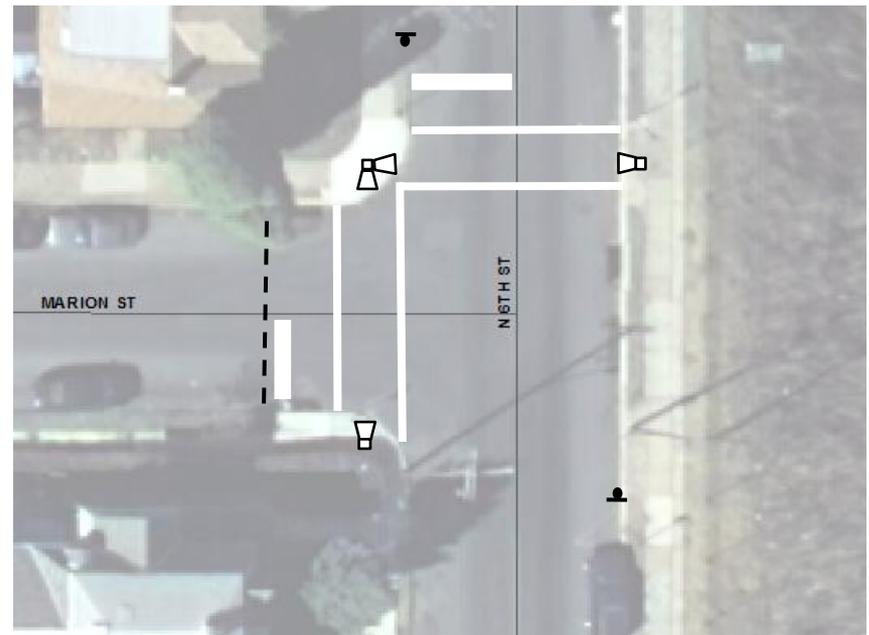
-  PEDESTRIAN CROSSING SIGN
-  CURB RAMP, DOUBLE WITH SHARED TURNING SPACE
-  CURB RAMP, SINGLE
-  SIDEWALK / ALLEY ENTRANCE REPLACEMENT SECTION
-  MILL & OVERLAY JOINT ON SIDE STREETS

MILL & OVERLAY SIDE STREETS 1 FOOT BEYOND THE CROSSWALK OR STOP LINE AND CONSTRUCT PAVEMENT JOINT (TYP).

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TYPICAL PLAN DETAILS
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TYPICAL CROSSWALK PLAN (6th ST. AND AMITY ST.)



TYPICAL CROSSWALK PLAN (N. 6th ST. AND MARION ST.)



TYPICAL CROSSWALK PLAN (6th ST. AND ROBESON ST.)

LEGEND

-  PEDESTRIAN CROSSING SIGN
-  CURB RAMP DOUBLE, WITH SHARED TURNING SPACE
-  CURB RAMP, SINGLE
-  SIDEWALK / ALLEY ENTRANCE REPLACEMENT SECTION
-  MILL & OVERLAY JOINT ON SIDE STREETS



W11-2*
PEDESTRIAN CROSSING SIGN

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TYPICAL PLAN DETAILS
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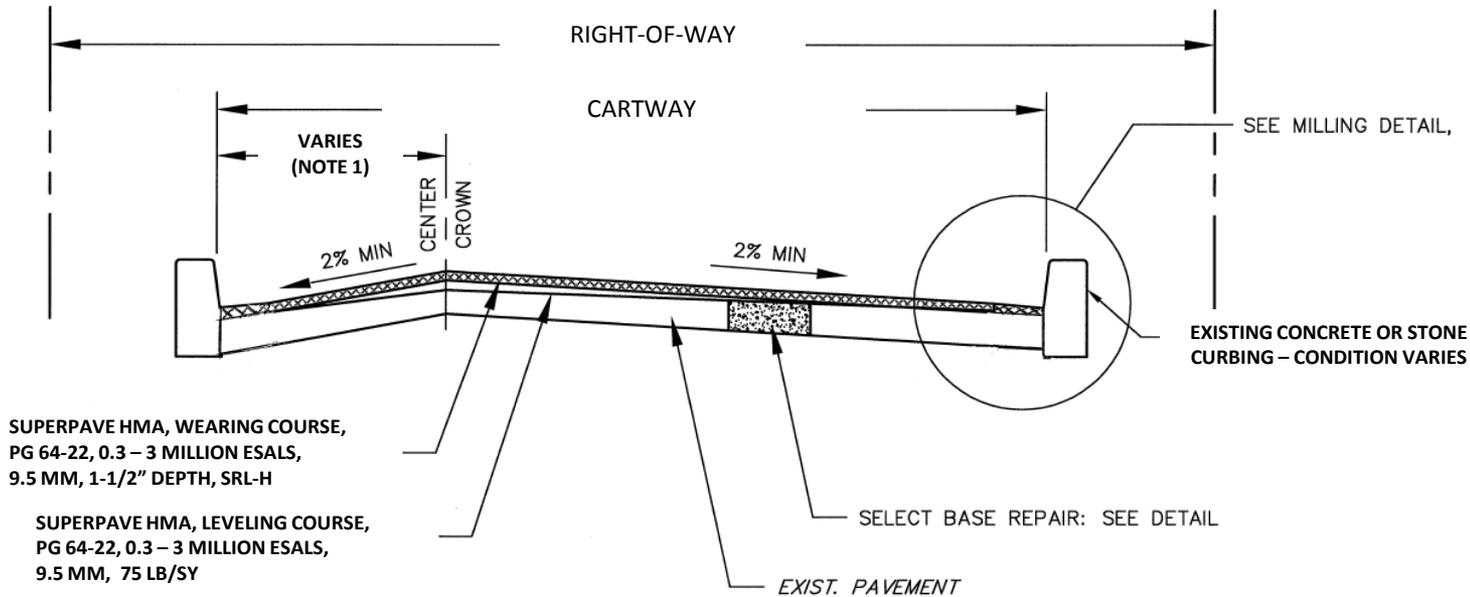


TYPICAL ALLEY ENTRANCE RECONSTRUCTION ALONG N. 6th ST.

LEGEND

-  PEDESTRIAN CROSSING SIGN
-  CURB RAMP DOUBLE WITH SHARED TURNING SPACE
-  CURB RAMP SINGLE
-  SIDEWALK / ALLEY ENTRANCE REPLACEMENT SECTION
-  - - - MILL & OVERLAY JOINT ON SIDE STREETS

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TYPICAL PLAN DETAILS
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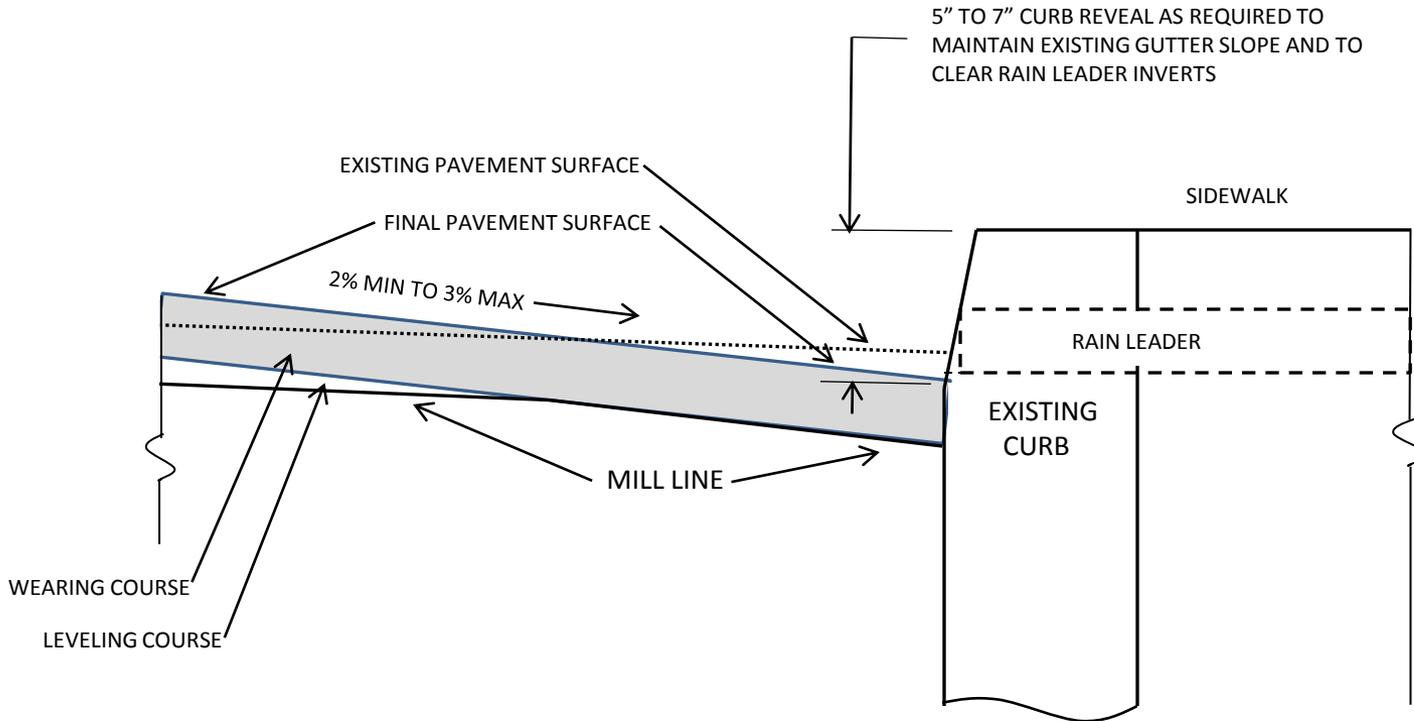


TYPICAL PAVEMENT OVERLAY SECTION

NOTES:

1. CROWN LOCATION WILL VARY DEPENDING ON THE DIFFERENCE IN CURB HEIGHTS. LOCATE THE CROWN AT THE CENTER OF THE CROSS SECTION WHERE THE CURBS ARE AT THE SAME ELEVATION. WHERE THE CURBS DIFFER IN ELEVATION, SHIFT THE CROWN TOWARD THE HIGHER CURB TO PRODUCE A 2% MIN. TO 3% MAX. CROSS SLOPE AND A MIN. 5" CURB REVEAL .
2. TACK COAT SHALL BE APPLIED ON ALL EXISTING PAVEMENT EDGES AND EXISTING PAVEMENT SURFACES PRIOR TO PLACING ANY NEW ASPHALT COURSE.
3. THE OVERLAY WIDTH WILL EXTEND TO THE OUTSIDE OF THE CURB RADIUS AT EACH INTERSECTING STREET. CONSTRUCT A PAVEMENT JOINT BETWEEN THE CURB TANGENTS AT EACH INTERSECTING STREET. MILL (OR SAW CUT) JOINTS AND OVERLAY TO MATCH THE EXISTING PAVEMENT.
4. SEAL CURB / PAVEMENT JOINT WITH

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MILLING AND OVERLAY SECTION
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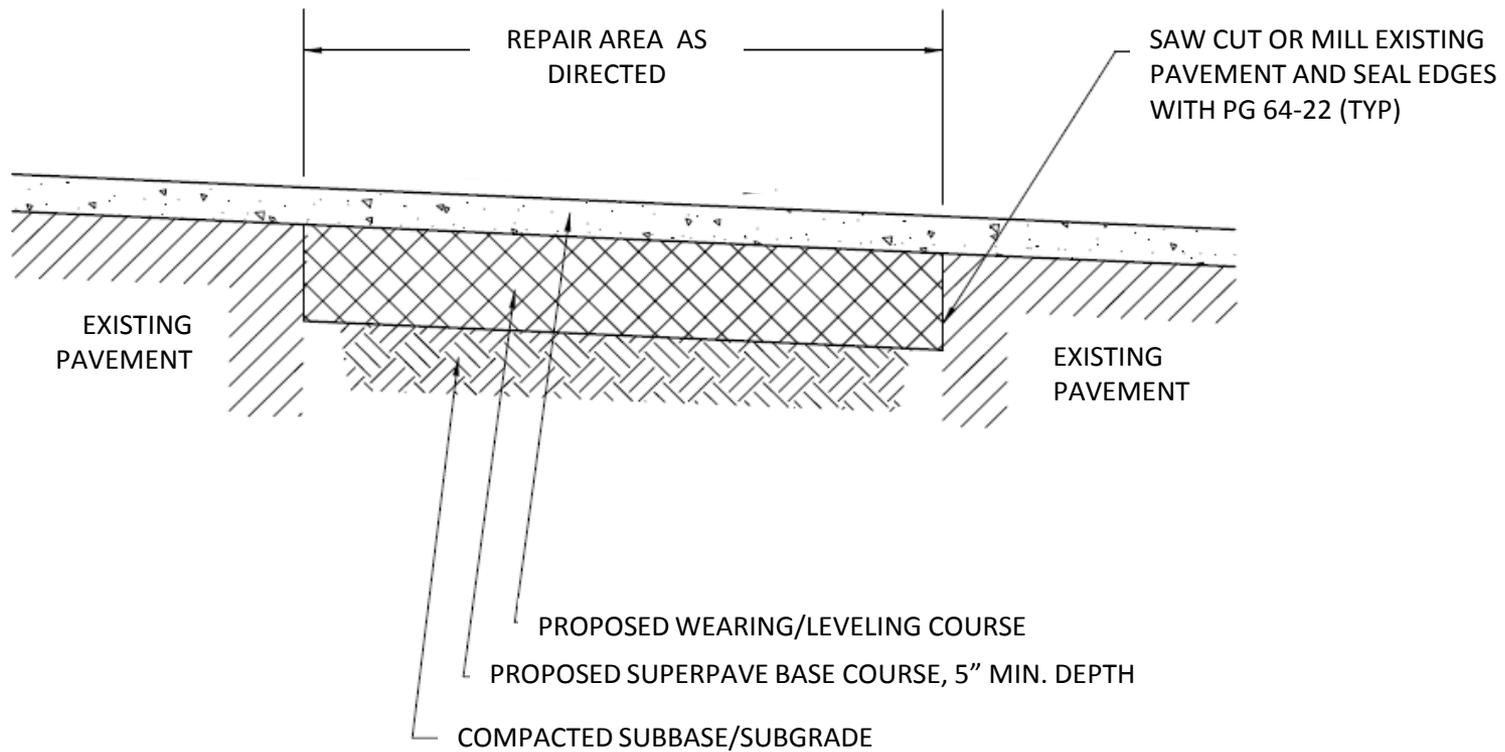


MILLING AND OVERLAY DETAIL

NOTES:

1. THE CURB GUTTER GRADE SHALL BE RESTORED TO ENSURE PROPER DRAINAGE. CONTRACTOR SHALL USE CAUTION WHEN MILLING AND PAVING ALONG CURB FACES TO AVOID DAMAGE TO THE CURBING.
2. THE LEVELING COURSE SHALL BE APPLIED TO RESTORE A FLAT LONGITUDINAL STREET PROFILE AND TO PROVIDE FOR A SMOOTH TRANSVERSE GRADE FOR PROPER DRAINAGE.
3. UTILIZE PENN DOT STANDARD DETAIL RC-28M TO CONSTRUCT OVERLAY TRANSITION JOINTS WITH EXISTING PAVEMENTS AND FOR LONGITUDINAL NOTCHED WEDGE JOINTS.
4. SEAL CURB / PAVEMENT JOINT WITH PG 64-22, 12" WIDTH.

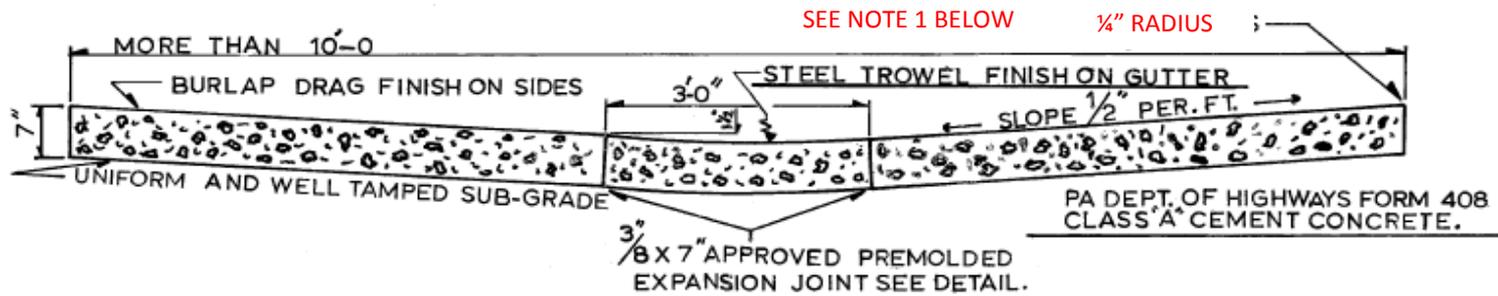
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MILLING AND OVERLAY DETAIL
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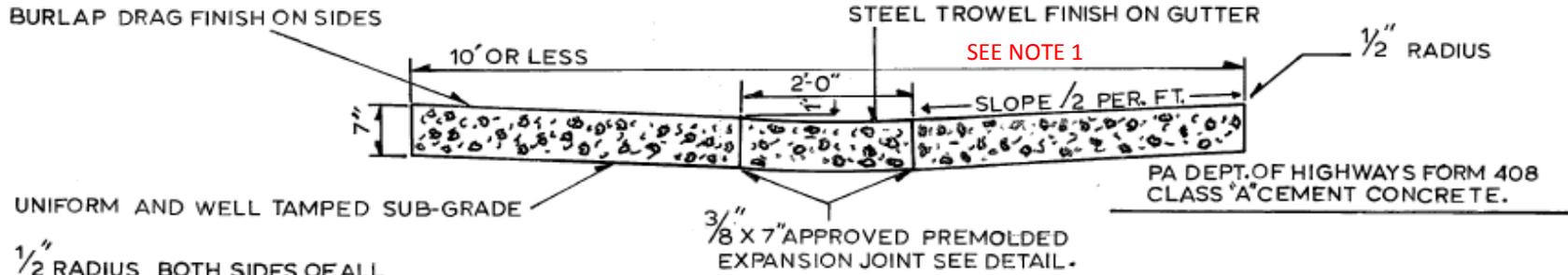
SELECT BASE REPAIR

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BASE REPAIR DETAIL
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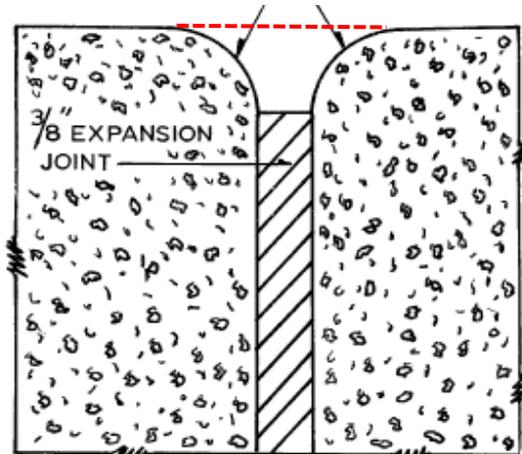
NOTE 1: THE LONGITUDINAL SLOPE (IN THE DIRECTION OF THE SIDEWALK) SHALL NOT EXCEED 4.5%. THE CROSS SLOPE (IN THE DIRECTION OF THE ALLEY) SHALL NOT EXCEED 1.7%. CROSS SECTION SHALL BE PARABOLIC AND THE DEPTH AT THE CENTER SHALL BE 4" - 6", OR AS DIRECTED BY THE CITY.



1/2" RADIUS BOTH SIDES OF ALL EXPANSION JOINTS.

1/4" RADIUS BOTH SIDES OF ALL EXPANSION JOINTS. CAULK ALL JOINTS FLUSH.

TYPICAL CROSS-SECTIONS
SCALE 1"=2'-0"



EXPANSION JOINT DETAIL

SCALE 12"=1'-0"

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STANDARD ALLEY CROSS SECTION
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STANDARD CROSS SECTIONS OF ALLEYS

CITY OF READING, PENNA.

BUREAU OF ENGINEERING

DEPT. OF STREETS & PUBLIC IMPROVEMENTS

Field Book No. _____ Page _____	Ordinance No. _____	APPROVED BY <i>Joseph T. Mitchell</i> CITY ENGINEER	PLAN No. 5390
Drawn _____	Date Passed _____		SHEET 1 OF 1
Traced <i>B.J.</i>	Superseded by _____	<i>Edward J. Bonnell</i> DEPT DIRECTOR	
Checked _____	Plan No. _____		
Date _____			